



CARRINGTON ROAD IMPROVEMENTS PROJECT

HERITAGE IMPACT ASSESSMENT

February 2025

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PART ONE OUTLINE OF PROPOSAL

1 INTRODUCTION

1.1 BACKGROUND

Auckland Transport has outlined a proposal to widen Carrington Road which runs between the intersection of New North Road and Mt Albert Road at its southern end, through to the Northwestern motorway and the Point Chevalier shops at its northern end. Along the western side of Carrington Road between Woodward Road and the motorway is a large area of land previously occupied by a hospital for the mentally unwell, variously known as the Auckland Provincial Lunatic Asylum and later, Oakley and finally, Carrington Hospital.

After the hospital closed, the land was purchased by the tertiary institution known as Unitec which constructed several new buildings to the south of the site and also occupied a number of the old hospital buildings which were located towards the north. In recent times, Unitec has consolidated at the southern end of the site and divested itself of most of the land to the north.

At the northern extremity of the site is the original main hospital building which was constructed progressively between 1864 and 1905. The building is mostly intact other than the recent removal of two later sections of the central and eastern wings. Elsewhere on the hospital site, other former hospital buildings have either been demolished or are about to be to make way for housing.

The main hospital building is the only structure on the site which is protected by way of it being scheduled in the Auckland Unitary Plan. Associated with the scheduled historic heritage building is an Extent of Place.

Along part of Carrington Road, towards its northern end is a two-metre-high brick wall which was built in 1887 as part of extensions to the eastern / female wing of what was then the Auckland Provincial Lunatic Asylum. The wall, along with the building bounded an area used for outdoor activities, referred to an Airing Court.

It is now proposed to remove the Airing Court wall to facilitate the widening of Carrington Road, which may impact on the heritage values of the site as the wall in question is located within the Extent of Place. The road widening will result in permanent works within the Extent of Place.

1.2 FORMAT AND PURPOSE OF REPORT

This report is in three parts. The first part provides a background historical account of the surrounding area including what is now Carrington Road. It includes a physical description of Carrington Road and then outlines the work that is proposed along its length.

The second part of the report focuses on the impact of the works on the heritage values of the former Carrington Hospital site, specifically arising from the removal of the wall. It includes a historical account of the brick wall and a summary of its heritage values.

The final section discussed the impacts of the proposed works on the heritage values of the Carrington Hospital site and concludes with suggested actions that could mitigate any potential loss of heritage values.

1.3 HERITAGE PROTECTION

Auckland Unitary Plan

The Oakley Main Hospital Building/former Carrington Hospital is the only structure on the site which is individually scheduled in the Auckland Unitary Plan Schedule of Historic Heritage (14.1) where it appears as a Category A historic heritage place (list number 01618). Category A places are considered to be of outstanding significance well beyond their immediate environs. Criteria under which the building is listed are historical, social, physical attributes, aesthetic and context.

While the former hospital is the only scheduled item, it has an extent of place associated with it as shown below. The Extent of Place relating to a scheduled item is defined in the Auckland Unitary Plan as follows.

The extent of place comprises the area that is integral to the function, meaning and relationships of the place and illustrates the historic heritage values identified for the place.

Exclusions to the scheduled item are listed in Schedule 14.1 as follows: All buildings and structures constructed after 1905, whether attached to the Oakley Hospital Main Building or freestanding; all vegetation within the extent of place; all post 1905 modifications to the interior of the Oakley Hospital Main Building.

The Airing Court wall with its construction date of 1887 predates the 1905 exclusion date and is therefore protected by way of its inclusion within the Extent of Place.



Figure 1 The heritage Extent of Place associated with the former Carrington Hospital in Schedule 14.1 of the Auckland Unitary Plan is represented in the purple crosshatch. Source: Auckland Council.

Heritage New Zealand

The former Carrington Hospital is listed by Heritage New Zealand Pouhere Taonga as a Category 1 Historic Place (List number Private/No Public Access 96). It was registered on 3 March 1986.

The entire former hospital site may be considered to be an archaeological site as it was a place of human activity prior to the year 1900. It may therefore be afforded protection as such under the *Heritage New Zealand Pouhere Taonga Act 2014*, which defines an archaeological site as: “A place associated with pre-1900 human activity, where there may be evidence relating to the history of New Zealand”.

1.4 COMMISSION AND AUTHORSHIP

This Heritage Assessment was commissioned by Auckland Transport and written by Dave Pearson, Principal of DPA Architects, with assistance from Debra Millar, researcher and writer at DPA Architects. The site was visited on Monday 21 October 2024.

1.5 INFORMATION SOURCES

Sources consulted in the preparation of this report are as noted and are listed in the Bibliography. The sources of historical and contemporary photographs are also noted.

2 PROPOSED WORKS

2.1 OVERVIEW OF WORKS

The works that are the subject of this report are concerned with widening Carrington Road and are detailed in a set of Preliminary Design drawings produced by Beca entitled Carrington Road Improvements Project, dated 18 December 2024.

Carrington Road is a 1.6km-long arterial road on the Auckland isthmus which connects New North Road at the Mt Albert Town Centre in the south; and Great North Road at the Point Chevalier Town Centre in the north. Auckland Transport (AT) has proposed the Carrington Road Improvements Project (CRIP) to serve planned growth and intensification in the area; and to achieve the long-term strategic network outcomes for the corridor, particularly a higher level of service for active modes and public transport.

To these ends, the CRIP comprises the following road upgrades which include a section of widening on the western side of the road between Woodward Road and State Highway 16 (SH16):

- Bus/special vehicle lanes (TBC) for most of the corridor length in both directions, and new/relocated bus stops;
- Improved walking and cycling facilities along the entire corridor length in both directions, new midblock crossings, and a new pedestrian bridge to supplement the existing Mt Albert Rail Bridge;
- Upgraded intersections along the entire corridor length, including four new/upgraded signalised intersections,
- New stormwater management infrastructure, including treatment and conveyance swales on Segar Avenue; and
- Public realm placemaking/landscaping, and new street trees.



Figure 2 Project Extent

In conjunction, Watercare Services Limited (Watercare) has proposed the Point Chevalier Watermain No. 2 Project (the Watermain) along Carrington Road. The Watermain is a Ø750mm concrete-lined steel (CLS) pipeline approximately 1km in length between Seaview Terrace and Sutherland Road, and forms part of a wider scheme to improve supply, maintain levels of service, and provide resilience to both the Point Chevalier and Khyber water supply zones. The design and planning for the Watermain has been expedited to realise efficiencies with the CRIP, and to enable the projects to be constructed concurrently.

Unless otherwise noted, the CRIP and Watermain projects are referred to collectively in this report as 'The Project'. The Project extent is shown at Figure 2. A full Project Description can be found in Section 3 of the Assessment of Effects on the Environment (AEE) report.

As noted, there is currently a historic brick wall dating from 1887 wall located adjacent to the Oakley Main Hospital Building which runs part of the way along Carrington Road. The wall is required to be removed to allow the road to be widened. Further to the south are miscellaneous short sections of stone walling which are not believed to have heritage value.

2.2 REMOVAL OF AIRING COURT WALL

The historic wall that is proposed to be removed is not listed as a Primary Feature but is located within the Extent of Place associated with the former Carrington Hospital. The proposal is to widen the road near the Oakley Main Hospital Building on its western side, rather than the eastern side. The reasons for this are set out in a Technical Memorandum prepared by Beca and dated 12 November 2024. In summary, these include the following:

- The widening of Carrington Road has been provided for as part of the Auckland Unitary Plan (Operative In part) Wairaka Precinct since 2016 via a boundary setback provision applying between Woodward Road and SH16. This requires new buildings to be set back 28.2m from the eastern edge of Carrington Road, providing for approximately 7-8m of road widening on the western side of the road.
- The boundary setback provision encroaches into the Oakley Hospital historic heritage extent of place, including the primary feature (the main hospital building) as well as non-primary pre-1905 features within the extent of place (including the Airing Court Wall).
- Three options were assessed at the short list stage, and all were assessed as requiring removal of the Airing Court Wall. These options included different configurations for cycling (bi-directional on one side or uni-directional both sides), and bus lanes (northbound only or both directions). The EPO includes uni-directional cycle lanes on both sides and bus lanes in both directions.
- While the 28.2m boundary setback provision cuts across the Oakley Hospital historic heritage extent of place, the EPO was designed to avoid impacts on the main hospital building as the primary feature – this was achieved by a localised narrowing of the widened corridor (to approximately 25m) and realigning to the east while remaining within the existing road reserve on the eastern side.
- Further realignment would likely require designation and partial acquisition of property to the east, including the Te Whatu Ora site directly opposite to the Airing Court Wall. This site contains a rehab centre and Marae, and it is likely that several large mature native trees would need to be removed from the site. Further private property may be required to realign affected intersections and integrate with the SH16 bridge to the north. Given these impacts, it is unlikely a further eastward option would perform well in an alternatives assessment (which would be required to designate the land); and the property requirements would present significant programme risks.

- The case to designate the land from a reasonable necessity perspective would be tenuous given that the Wairaka Precinct boundary setback provision otherwise already provides for widening of up to 7-8m on the opposite side of the road.
- Narrowing of the corridor beyond the localised narrowing already incorporated into the EPO without further widening on the eastern side would result in the road remaining at its current width of ~20.5m, which is insufficient to accommodate the transport outcomes sought in this location.
- Along the eastern side of Carrington Road is a prominent grove of semi-mature native Pohutukawa trees. Some of these may have to be removed if the road widening was to occur on the eastern side. This could have a significant impact on the character of Carrington Road. Under the scheme as proposed, the Pohutukawa trees are all able to be retained.

A corner of the female section of the former hospital building which dates from 1909 will now be located within the extended road reserve. It is not proposed to remove any part of the building which will mean that the footpath and cycle lane will need to “dogleg” around the end of the building.

2.3 OTHER ITEMS WITHIN THE EXTENT OF PLACE

To the north of the Airing Court wall and continuing along the Carrington Road boundary towards Great North Road is a low brick wall which was likely constructed to replace a timber post and rail fence seen on early photographs such as that reproduced as Figure 7. The provenance of this wall is not known, although it appears to be of recent construction and certainly later than 1905.

Further to the north is a single storey building, the original section of which was constructed in 1946 as the New Zealand Occupational Therapy Training School. It has no protection and is also marked for demolition. Also proposed for demolition is a small single storey brick building to the south of the main block which is believed to have functioned as a staff laundry. This also is a recent structure and is considered to have no heritage values. It has no protection which would prevent it from being demolished. Each of these buildings will be demolished prior to the construction of the Project as part of the Carrington Residential Development works.



Figure 3: Photograph looking south. On the left are the Pohutukawa trees on the eastern side of the road. On the right is the end of the former hospital behind the Airing Court brick wall is obscured by creepers.



Figure 4: View also looking south along Carrington Road. To the right is part of the main hospital building and the later brick wall.

2.4 NEW WORKS WITHIN THE EXTENT OF PLACE

Included in the project are works within the Carrington Hospital Extent of Place. These include the following:

- New footpaths, cycle lanes and shared paths.
- Plantings including trees, grassed berms and rain gardens.
- New stormwater lines.
- New streetlights and associated cabling.
- Short section of new watermain and new valve chamber recessed into the ground.

The impact of these works will be assessed later in this report in Sections 8 and 9.

3 DEVELOPMENT OF CARRINGTON ROAD

3.1 EARLY DEVELOPMENT

Carrington Road, which was originally known as Gladstone Road, appears on a map dated 1892 (Figure 5) showing residential lots along its eastern side and the asylum to the west. At that time, the road follows roughly the same line as today, extending from the intersection of Great North Road and Pt Chevalier Road in the north to the intersection of New North Road and Mt Albert Road in the south.

The opening of the Auckland Provincial Lunatic Asylum in 1867 no doubt provided an impetus for the road's development, with a bus service operating on Great North Road between the city and the asylum from the late 1880s which had the effect of stimulating residential growth in nearby suburbs. By 1887, the first Pt Chevalier District School (now Gladstone Primary School) was located on Gladstone Road, so called because Point Chevalier residents had demanded their children be spared a two-mile trek to either Morningside or Avondale.¹

Gladstone Road also served as an important early road corridor to the Kaipara rail line, which opened in 1880, with the Mount Albert Railway Station positioned close to the intersection of Gladstone and New North roads. The connection to the rail line provided impetus to the surrounding area's early development and, in 1884, the 36-acre Benfield Estate near to the rail station was offered for sale as a single lot, with the provision that if it was sold as a whole, it should then be on-sold as smaller residential sections.²

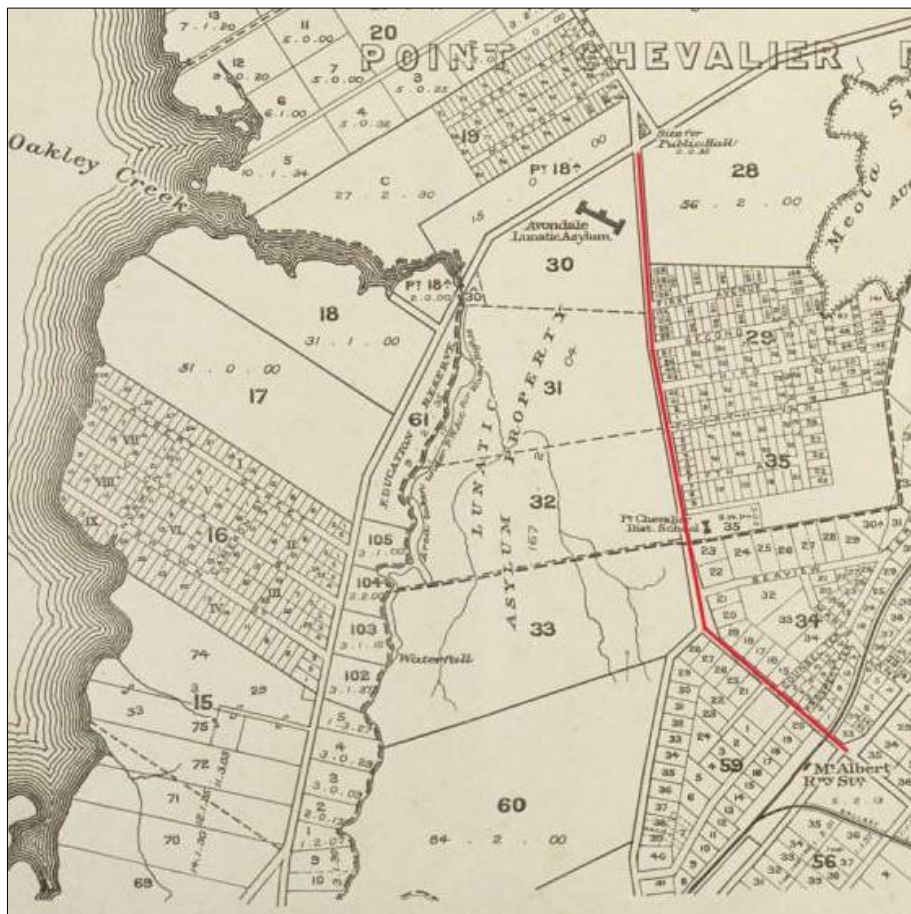


Figure 5 1892 map showing Gladstone Road with residential lots. Source: Auckland Libraries Heritage Collections Map4785.

¹ Debra Millar, *The Point: a history of Point Chevalier and its people*, 2019, p121

² Albert-Eden Heritage Survey, Auckland Council, September 2013, p71



Figure 6 By the 1880s a bus service operated between the city and the asylum.
Source: Auckland Libraries Heritage Collections.



Figure 7 The asylum viewed from Great North Road and Gladstone Road (at left) c.1895.
Source: Auckland Museum PH-1965-4-B3455.

By 1910 Mount Albert was among the suburbs in the country with the most rapid growth and the extension of the city's electric tramline along New North Road to the Mount Albert town centre in 1915 provided an impetus for commercial development. The shopping centre at the corner of New North and Mt Albert roads had its beginnings in 1880s, when a general store and post office opened. By 1910 more shops had become established but commercial development flourished with the extension of the tramline. The construction of two-storey commercial buildings, with retail premises at street level and residences above, continued during the 1920s around what became known as 'The Terminus'.³ Many of those shops remain in use today and the predominantly inter-war Mount Albert town centre is among only a small number of intact commercial centres in Auckland from this period.

Well served with transport options and neighbourhood shops, Mount Albert became the fastest growing borough in the country in the 1920s, with houses being built at the rate of one and a half per month⁴. By 1936, the borough boasted a population of more than 20,000, making it the largest suburb in New Zealand. While early settlers in the area built villas and cottages, the 1920s' building boom resulted in a concentration of bungalows and arts and crafts-style houses. State houses were added as the first Labour Government's housing programme kicked into gear in the late 1930s. Carrington Road includes examples of all these types of housing.

The northern-most stretch of Gladstone Road as far as Woodward Road was renamed Carrington Road in 1938, while the remaining stretch of road through to New North Road retained the name Gladstone Road until the early 1960s.⁵ Photographs from the 1930s and 1940s show the northern of Carrington Road sparsely populated with housing. At this time, the Wolfe Bequest Home was located on the eastern side of the road opposite the main hospital building.

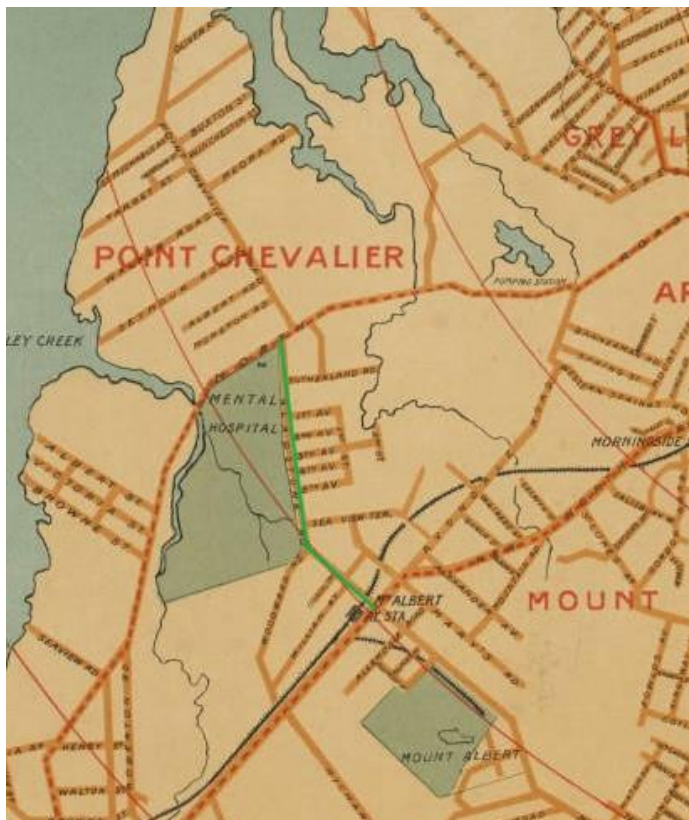


Figure 8 1915 Brett's map of Auckland showing Gladstone Road highlighted in green, rail line in blue with Mt Albert rail station and tramline dotted red. Source: Auckland Libraries Heritage Collections Map9801.

³ Ibid. p90

⁴ Ibid. p75

⁵ Historian Lisa Truttman writing about the road's history in Timespanner.blog.spot



Figure 9 Carrington Road in 1949. Source: Alexander Turnbull Library, Whites Aviation WA-20753.

By the 1960s, the eastern side of Carrington Road had been developed with housing, although Oakley Hospital continued to occupy land on the eastern side of the road at its northern end.



Figure 10 Carrington Road shown in red in 1961. Source: retrolens.co.nz

3.2 CARRINGTON ROAD CONTEXT

Present-day Carrington Road is an arterial route running broadly north to south and connecting Point Chevalier at its northern end to Mount Albert to the south. The road crosses over the Northwestern Motorway at its northern end and the Auckland west rail line at its southern end.

The former Carrington Hospital site, including a parcel of land retained by Unitec, is located to the west of the road between Woodward Road and the northwestern motorway. On the eastern side of this section of road is a mix of health facilities and commercial buildings towards the north. Housing occupies the eastern side of the road from Segar Ave to New North Road. Gladstone Primary School is located almost directly opposite Woodward Road amongst the housing. Between Woodward Road and New North Road, the western side of the road is also lined with housing up to the rail bridge that crosses the Auckland west rail line. Housing styles along Carrington Road include a mix of villas, bungalows and recent multi-unit developments.

Carrington Road Context



Looking south from the intersection of Carrington Road and Great North Road.



Looking north past Fontenoy Street (at right).



Looking north from Gate 4 entrance to Unitec site.



Looking north from entrance to Biomed commercial premises on east side of road.



Looking north from new Gate 1 entrance to former Carrington site.



Looking north from intersection of Carrington Road and Segar Ave.



Penman House (former Lodge) on the corner of Woodward Road and Carrington Road.



Looking south towards Mt Albert Road beyond Woodward Road.



Rail overbridge over Auckland west rail line.



Interwar period shops at the corner of Carrington Road and New North Road.

3.3 ARCHITECTURAL STYLES

Carrington Road displays a mix of housing styles, reflecting original development from the late 19th century through to mid-20th century and including some recent multi-unit developments that have replaced earlier dwellings. Houses are predominantly single level on full sites, with bungalows being the predominant housing style.

The late 19th century brick buildings associated with Oakley Hospital / Carrington Hospital on the western side of the road at its northern end continues to maintain an imposing presence. Currently, a multi-storey apartment building is being constructed immediately behind the original 1860s asylum building as part of a large urban residential development encompassing most of the former hospital site. Additional apartment buildings are planned for the western side of the road.

Unitec is located at the southern end of the former hospital site. The Unitec buildings are located below the level of the road and therefore out of view and are approached via a roadway

accessed from Gate 4. Buildings housing Unitec were mostly developed during the 1970s by the Carrington Technical Institute.

Between Great North Road and Fontenoy Street, opposite the former hospital site, is a concentration of state housing, including a three-level block of flats recently developed by Kainga Ora on the corner of Tasman Avenue. Directly opposite these flats is a red brick villa believed to be one of the earliest dwellings at this end of Carrington Road.

Bungalows predominate between Woodward Road and the New North Road end of Carrington Road, with some villas, located mainly on the eastern side of the road, reflecting earlier development.

3.4 SITES OF INTEREST

The former Carrington Hospital site is the only site potentially affected by the proposed widening of Carrington Road that is protected by way of it being scheduled in the Auckland Unitary Plan. The former hospital is also listed by Heritage New Zealand Pouhere Taonga.

Penman House on the corner of Carrington and Woodward Road is included in the Cultural Heritage Inventory compiled by Auckland Council although this does not convey any degree of protection.

Carrington Road is also of interest as it also contains a snapshot of residential architectural styles ranging from Victorian and Edwardian villas and continuing through the bungalow and State House periods. In later years, the area has seen the development of multi-unit residential complexes following the removal of individual houses.

Also of interest are the interwar commercial developments in Great North Road and New North Roads at the northern and southern end of Carrington Road and the Western Rail Line which was completed in February 1880.

None of these sites are impacted by the proposed widening of Carrington Road.

3.5 OTHER ITEMS

Along the length of Carrington Road are other items that could potentially be of interest. These include a basalt stone wall at the intersection of Carrington and Farm Roads which is now the main vehicle entrance to Unitec and other shorter section of stone wall located further to the north. Extending from the airing court northwards to SH16 is the low brick wall, referred to in Section 2.1. The provenance of this wall is not known.



Two section of stone walling (above) and brick wall (below) along Carrington Road.



None of these items are considered to have heritage values, although the brick wall is located within the Extent of Place.

Residential Architectural Styles Along Carrington Road

**Clockwise from top left:**

A brick villa on the corner of Tasman Ave; a weatherboard villa; bungalows including a modified example; a state house and recent multi-unit development by Kainga Ora; Penman House, on the corner of Woodward Ave, a double-storey house in the English domestic revival style built for the hospital's medical superintendent in 1909; a dwelling likely originally constructed as a State house and an English style bungalow.

PART TWO: ASSESSMENT OF HERITAGE VALUES

4 FORMER CARRINGTON HOSPITAL

4.1 OVERVIEW

As noted, the land on the western side of Carrington Road between the Great North Road and Woodward Road was formerly occupied by an institution originally known as the Auckland Provincial Lunatic Asylum. The initial section of the main building located at the northern end of the site was constructed in 1864 and it was progressively extended over the next 45 years with the final section being constructed in 1909.

One of the early sections of the hospital, dating from 1887, comprised an extension to the female wing at the eastern end. In conjunction with this, a brick wall was constructed that extended out from the southern end of the building, continued along Carrington Road and returned back to the building at its northern end. The area behind the wall was labelled as an airing court, which allowed the patients to have access to fresh air. The sections of the wall extending out from the building have since been demolished, although the section along Carrington Road remains.

The remaining section of wall is now proposed to be demolished to enable Carrington Road to be widened. The end of the adjacent building will now project into the extended road reserve but is not proposed to be modified, rather requiring the footpath and cycle lane to “dogleg” around it.

As described below, the building is scheduled as a historic heritage place and the wall is included in an associated Extent of Place. This part of the report includes a brief history of the former hospital and the wall. The final part of the report assesses the impacts caused by the removal of the wall and new permanent works on the heritage values of the place; and suggests potential ways of mitigating any negative impacts.

4.2 LEGAL DESCRIPTION AND OWNERSHIP

The land occupied by the Oakley Main Hospital Building/ former Carrington Hospital including the brick wall has a legal description of Lot 5 DP 314949).

The wider hospital site encompasses land held by the Crown, Unitec New Zealand, Health New Zealand Te Whatu Ora, and Ngāti Whātua Whai Rawa. A substantial portion of the site is currently being developed as a large-scale residential housing development led by the three Tāmaki Makaurau rūpū of Marutūāhu, Ngāti Whātua and Waiohua-Tāmaki (the Carrington Residential Development).

5 HISTORICAL BACKGROUND

5.1 DEVELOPMENT OF THE HOSPITAL

By the early 1860s, Auckland's mentally ill were being detained in a building that formed part of the city's main hospital on Grafton Road. However, overcrowding resulted in the Auckland Provincial Council seeking an alternative site for a purpose-built asylum and 25 acres of land on Mount Albert Road was initially set aside for this purpose. The asylum was based on plans said to have been drawn up by an English architect, Mr Barrett,⁶ which were adapted by an Auckland architect, James Wrigley. Construction commenced in 1864 and by March 1867 the first patients had been moved from the main hospital.⁷

The brick asylum building initially consisted of a two-storey central administration and service wing with an entrance on its northern side, and one of two planned wings to the east, which contained patient dormitories. It was the largest public building in the Auckland province upon its completion.

At the time of its opening, it was known as the Auckland Provincial Lunatic Asylum, although it was often referred to as the Whau Asylum and sometimes as the Avondale Lunatic Asylum. By 1875, the inmate population was made up of 105 males and 57 females, with dormitories still contained within a single wing to the east.

In September 1877 a fire, which began in the upstairs female quarters, destroyed much of the first floor of the eastern wing. The fire and the obvious overcrowding at that time provided an incentive to finally complete the original design for the asylum. This involved the construction of a west wing, which was completed in 1881 and enabled patients to finally be segregated. Male patients occupied the west wing while females were housed in the rebuilt eastern wing.⁸ Despite the completion of the male patient wing, the overcrowding that had been experienced almost since the time of the asylum's opening remained a concern and it was extended further on several occasions until the early 20th century.



Figure 11. The main asylum building c.1895 after the addition of the west wing at right and reconstruction of the east wing at left. The roadway in the foreground is the Great North Road. The roadway at left is Gladstone Road. Note the white painted post and rail fence and wooden gates. Source: Margaret Matilda White, Auckland Museum PH-1965-4-B3480.

⁶ Salmond Architects, 'Former Carrington Psychiatric Hospital: a conservation plan', 1994

⁷ Clough & Associates, 'Wairaka Precinct: Archaeological and Heritage Due Diligence', November 2017, p8

⁸ Salmond Architects, 1994

One such extension in 1887 increased the capacity of the eastern female wing by adding new dormitories, ablutions and day rooms. At the same time, a brick wall was constructed between this extension and what was then known as Gladstone Road to create an airing court allowing patients to access fresh air and sunlight outdoors while remaining securely contained.

5.2 CONSTRUCTION OF THE AIRING COURT WALL

As noted, an ongoing need for additional accommodation led to a series of additions to the main asylum building between the mid-1880s and early 20th century. In 1886 the Government architect Charles E Beatson designed an extension to the female (eastern) wing of the asylum, which extended it further towards what was then Gladstone Road (now Carrington Road). The two-storey extension provided accommodation for an additional 100 patients as well as new ablutions and extra day rooms.

Beatson's elegant extension was designed in the Italianate style and carefully detailed to match the existing asylum building. Beatson's drawings for the extension also show a 'New Airing Court for Females' contained within a wall that ran parallel to the road and returned at right angles towards the main building at either end (Figure 12). A detail of the outside elevation of the wall and a drawing showing it in section are included with Beatson's drawings (Figure 13).

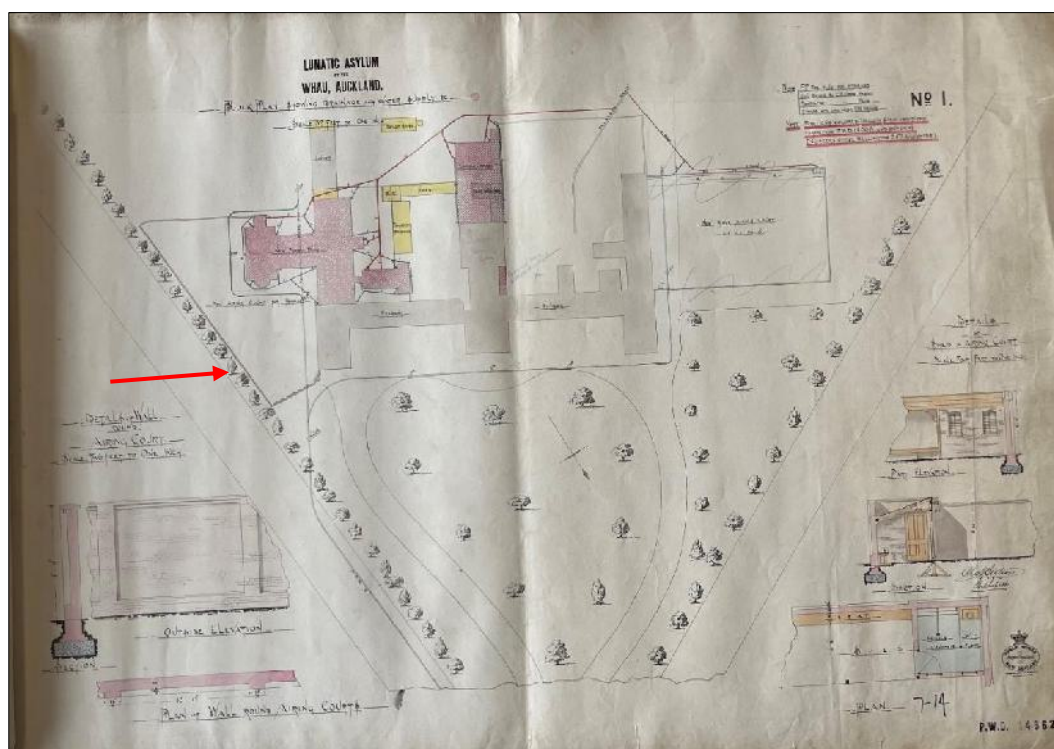


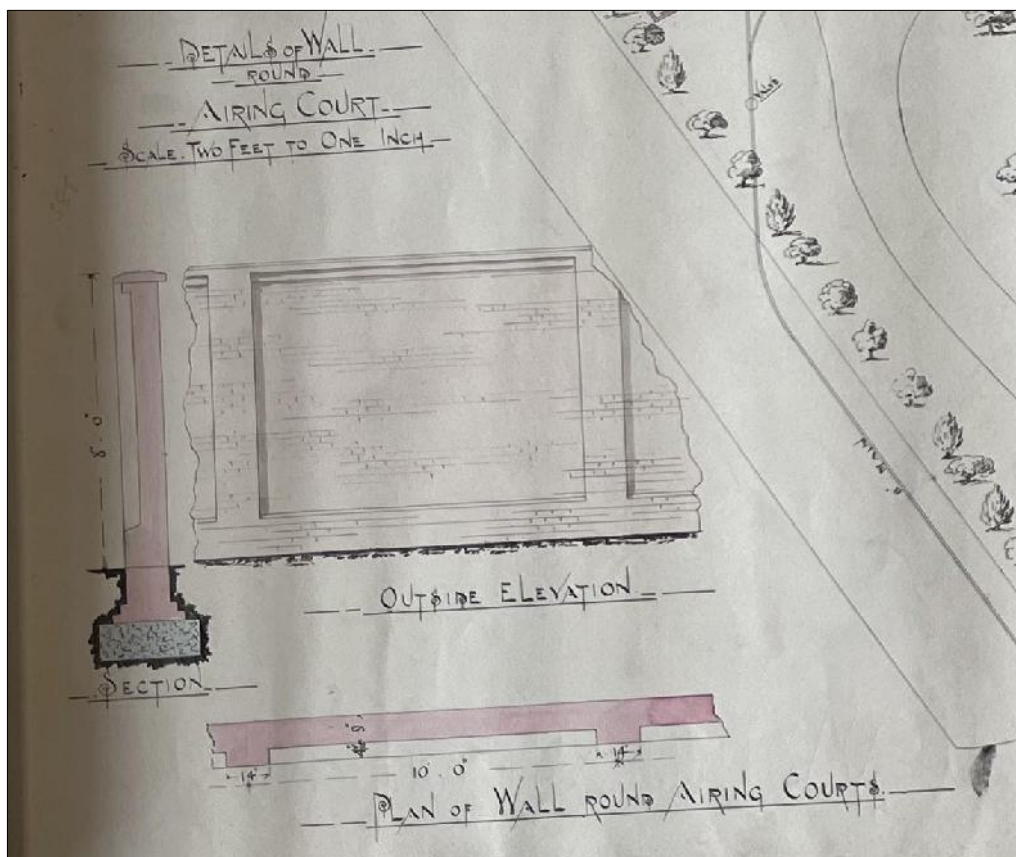
Figure 12 1886 drawing of extension to female (eastern) wing with walled airing court.
Source: Archives New Zealand Wellington repository R19471955.

An 1891 site plan of the asylum building also shows a wall matching Beatson's drawings and in the location of the current wall (Figure 14). Around 1903 the female wing was extended further towards the east with a single level addition accommodating 20 single rooms. At this point, the wall may have been modified to accommodate this extension.

The earliest photograph of the wall research has located is dated 1930 and shows it extended to the south towards a laundry building at the rear of the female wing (Figure 15). However, in this photograph the wall does not appear to return at right angles to the northern end of the eastern wing as shown in Beatson's earlier drawings.



Figure 13 The position of the wall containing the new airing court adjacent to what was then Gladstone Road, and an outside elevation of the wall by Charles Beatson (below).
Source: Archives New Zealand R19471955.



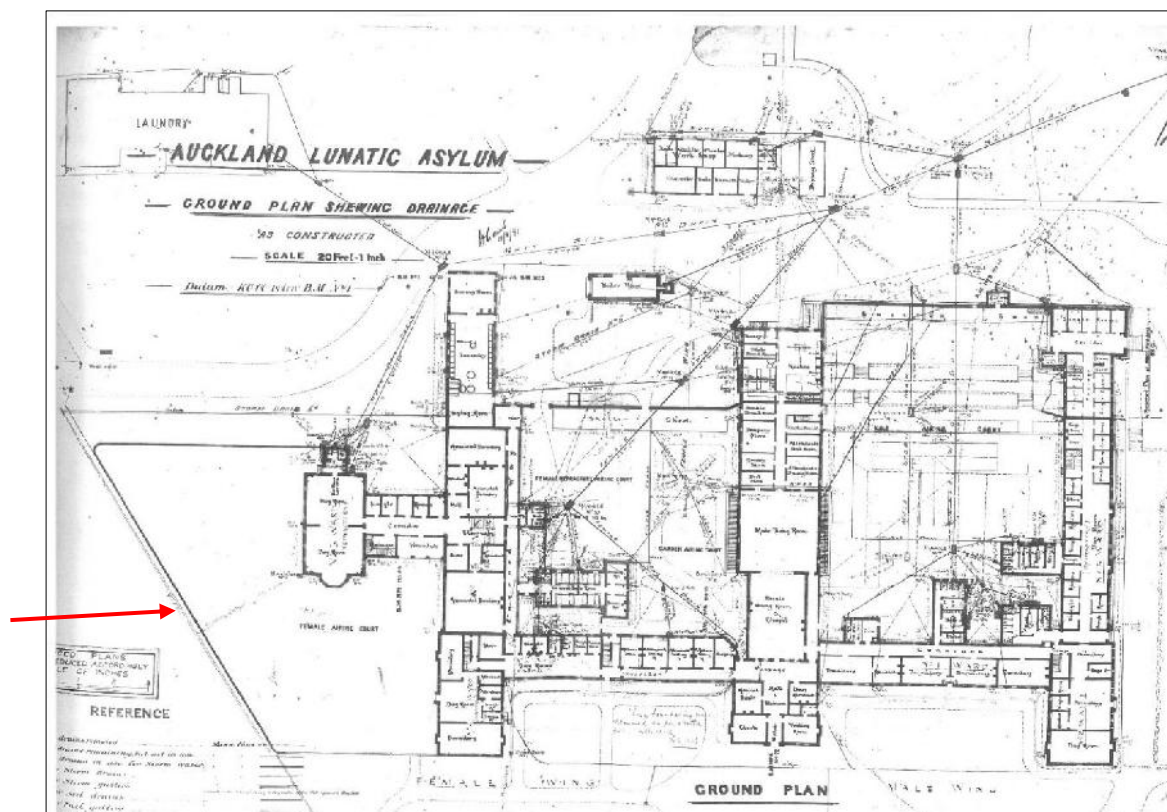


Figure 14 1891 site plan showing walled female airing court indicated with arrow.
Source: Archives New Zealand.

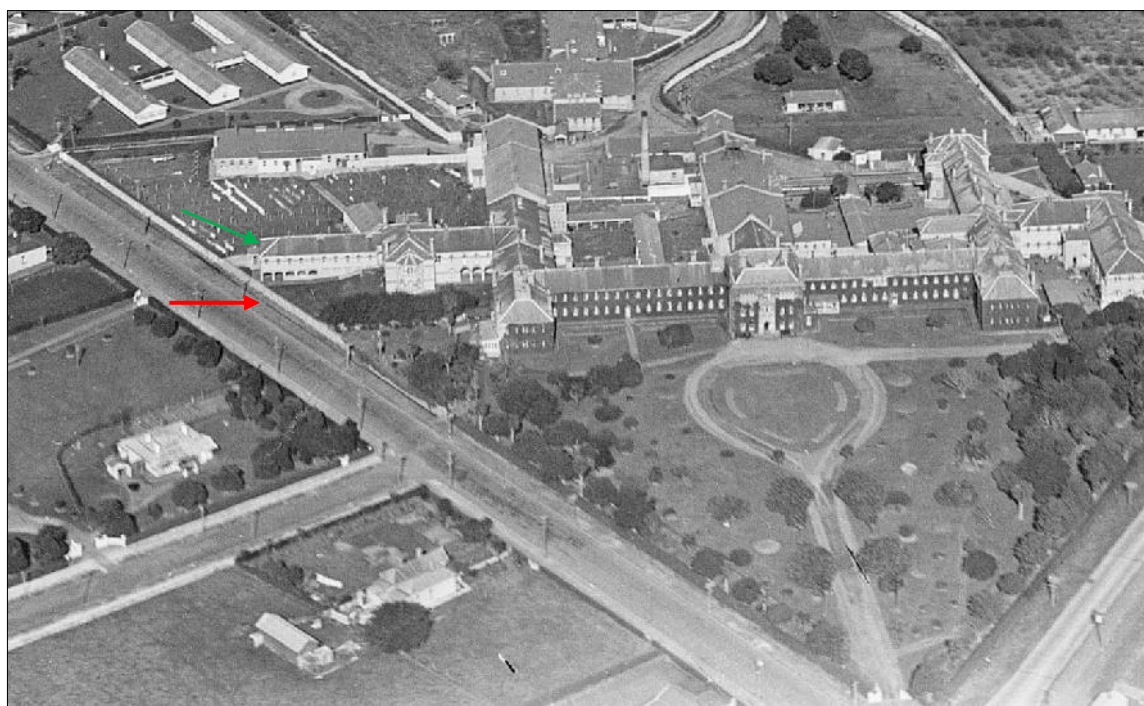


Figure 15 A 1930 photograph showing the wall extended to the south. The original section of wall is indicated with the red arrow. A 1903 extension to the east wing is indicated with a green arrow. By this time a laundry has been built to the rear of the female wing and a large drying court appears to be positioned between the two buildings. Source: Auckland Libraries Heritage Collections, F Douglas Mill Collection RDM-0515-G.

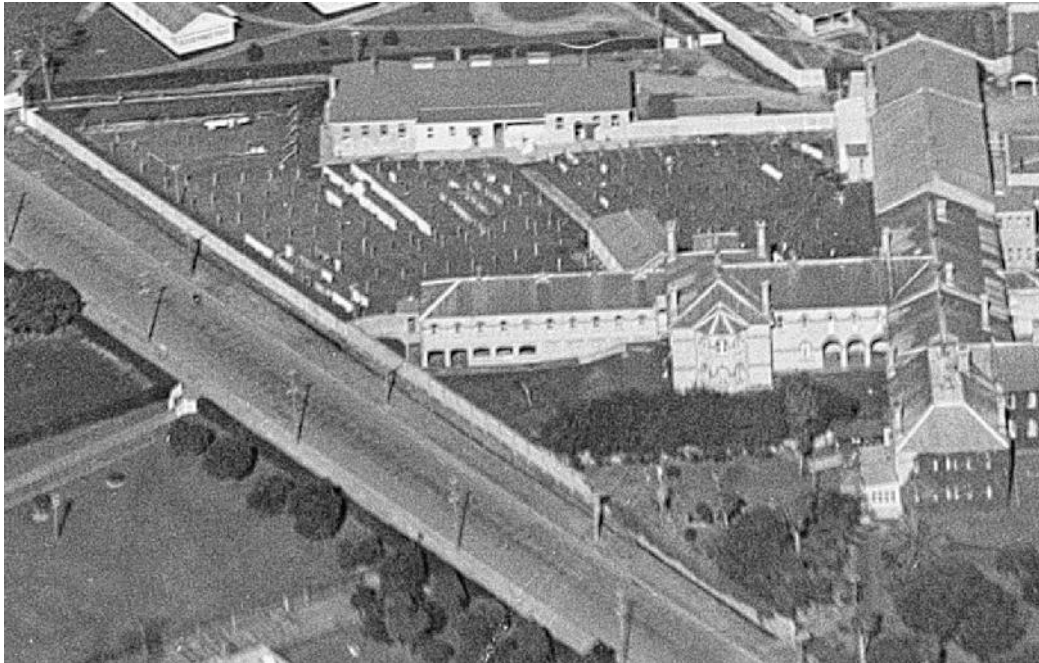


Figure 16 *In this close-up view of the wall, it does not appear to return towards the north end of the east wing as shown in the drawings at Figures 12 & 13.*

In 1946, a single level brick building was constructed to the north of the hospital's east wing adjacent to Carrington Road to accommodate the New Zealand Occupational Therapy Training School, the first such school established in New Zealand.

A 1949 aerial photograph (Figure 16) shows this building and a wall close to it that connects to the end of the main building's east wing. This is believed to be the existing low brick wall in this location, which is topped with metal railings. Although it is not in the same position as the 1887 wall shown close to this location in Beatson's drawings, the bricks and mortar in the existing wall (Figure 17) are a close match to the remnant of the 1887 wall parallel with Carrington Road.



Figure 17 A 1949 aerial photograph showing the Occupational Therapy Training School indicated with green arrow and what appears to be a wall in the location of the existing wall perpendicular to Carrington Road. Source: Alexander Turnbull Library, Whites Aviation WA-20753.



Figure 18 An existing low brick wall topped with metal railings that runs in an east to west direction towards the north end of the main building's east wing.

5.3 ASSOCIATED GROUPS AND INDIVIDUALS

Charles Beatson, architect⁹

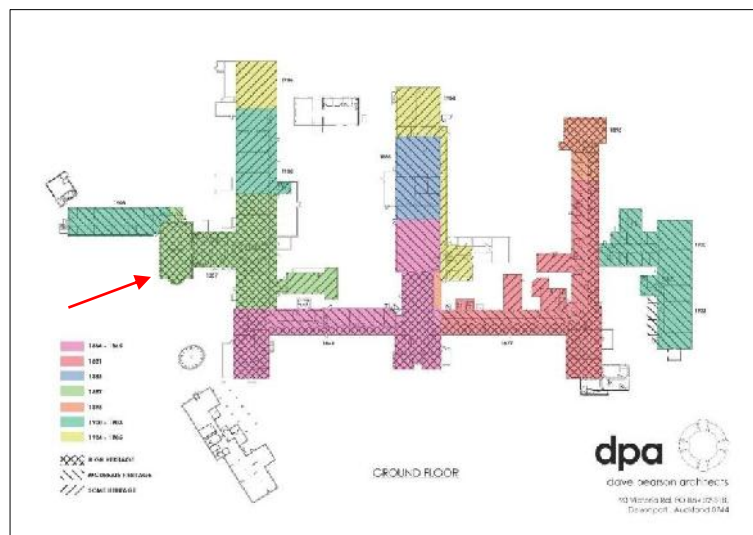
Charles Beatson was a son of the noted Nelson architect William Beatson, who died in 1871. He was articled to his father and worked firstly in the Nelson area where he supervised some of Beatson senior's work, including All Saints church, Nelson. He also undertook some of his own design work, including Holy Trinity, Richmond. In 1874 he was appointed assistant to the colonial architect, a post he held until 1886. He was subsequently Government Architect from 1886-88, during which time he was responsible for a number of public buildings. He died on 14 June 1927.

5.4 CHRONOLOGY OF EVENTS

1867	The Auckland Provincial Lunatic Asylum opens at Point Chevalier.
1877	A fire in the eastern wing of the asylum results in it being extended to the original design, with a western wing added and female and male patients segregated.
1887	Charles Beatson designs a two-level extension to the female wing which includes the construction of a walled airing court.
1903	The eastern female wing is further extended to the east, possibly altering the design of the walled airing court.
1930	An aerial photograph depicts the wall extended to the south to enclose an area between the female wing and a laundry, which appears to be used as a drying court.

5.5 CHRONOLOGICAL PLANS

The plan below records the dates of modifications made to the east wing that housed female patients. The extant brick wall is a remnant of an airing court constructed with the 1887 extension shown in green crosshatch indicated with arrow.



⁹ Salmond Architects, 'Former Carrington Psychiatric Hospital: a conservation plan', 1994, p35

6 PHYSICAL DESCRIPTION OF THE AIRING COURT WALL

6.1 SITE AND CONTEXT

The existing brick wall borders the former Carrington Hospital site on the western side of Carrington Road close to the recently reconstructed Gate 1 entry to the site. The wall is positioned next to a public footpath and is currently engulfed in vegetation for most of its length. A small, curved section of brick wall, visible at its southern end, marks the point where the airing court wall originally returned back to connect with the section of the main building's eastern most wing that was constructed in 1887.



The brick wall on the western side of Carrington Road and to the east of the former Carrington Hospital building. The building to the left was once in use as a laundry for patients. Source: Google maps.

6.2 ARCHITECTURAL DESCRIPTION

The wall was designed in 1886 at the same time as an Italianate style two-level addition to the eastern wing of the asylum, which housed female patients.

The wall is divided into rectangular panels by pilasters that terminate in an angled stringcourse that surmounts a base that is three bricks high. The top of the wall has a plastered brick cap. The rear of the wall does not feature the same rectangular panels.

The wall has been laid in an English Bond pattern, comprising alternating courses of headers (bricks laid perpendicular to the wall) and horizontal stretchers with the headers centred over the joints of the stretchers below. Although not determined for certain, it is likely that the bricks would have been made at one of the early Auckland brickworks that were located along the banks of the nearby Whau River.

The mortar is a dark reddy colour mix, possibly due to the use of a volcanic ash aggregate. Although the majority of brickwork dating from the period during which the wall was constructed would have used a lime mortar with the lime being produced by burning sea shells and the like, the mortar in the wall is surprisingly hard. This would suggest that cement was added to the mortar mix. Although not widely used, cement was being produced in the Auckland region in Warkworth from the 1880s. There remained, however, a preference amongst authorities for imported cement.



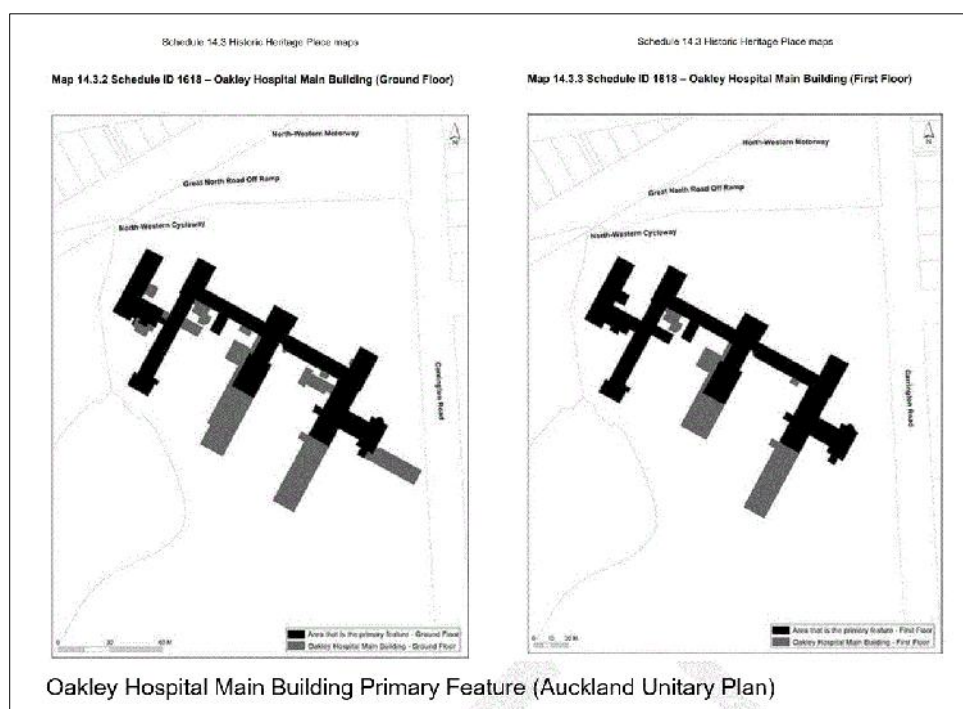
7 STATEMENT OF HERITAGE SIGNIFICANCE

7.1 SIGNIFICANCE OF THE BRICK WALL

The Oakley Main Hospital Building, also known as Carrington, is located on LOT 5 DP 314949 and is scheduled in the Auckland Unitary Plan as a Category A historic heritage place. The Unitary Plan defines a Category A historic heritage place as follows:

Category A Places: historic heritage places of outstanding significance well beyond their immediate environs; generally expected to be of significance to the Auckland region or a greater geographic area.

The area of the building that is identified as the primary feature is included in Schedule 14.3 of the AUP and is reproduced below. Excluded from the listing are all buildings constructed after 1905, whether attached to the Oakley Hospital Main Building or freestanding, all vegetation within the extent of place and all post 1905 modifications to the interior of the building.

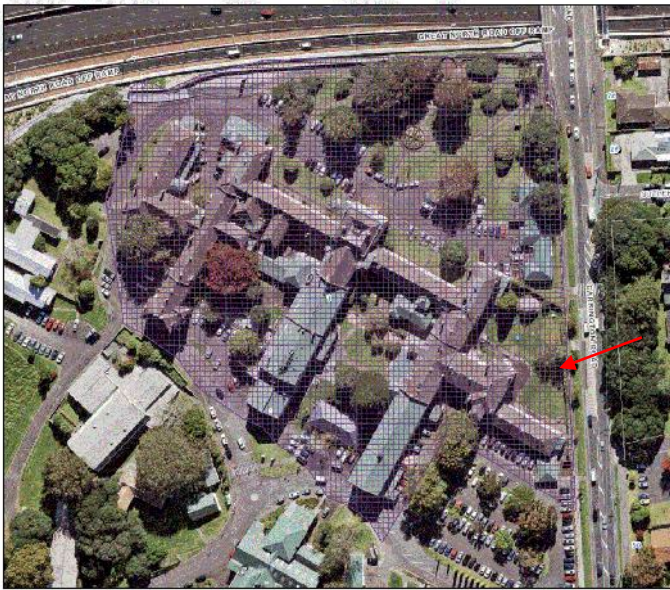


The Heritage values the former hospital has been assessed as having are: A, Historical; B, Social; F, Physical Attributes; G, Aesthetic and H, Context.

The brick wall along Carrington Road is not specifically included as part of the scheduled item but is within an associated Extent of Place. Section D.17 of the Auckland Unitary Plan defines an extent of place as follows.

The extent of place comprises the area that is integral to the function, meaning and relationships of the place and illustrates the historic heritage values identified for the place. The provisions relating to a historic heritage place apply within the area mapped as the extent of place on the Plan maps, including the airspace.

Although the brick wall is not part of the primary feature on the Carrington site its location within the Extent of Place means it is afforded the same level of protection as the building.



*Oakley Hospital Main Building Extent of Place. Carrington Road is shown at right of this image with the red arrow indicating the brick wall:
Source Auckland Unitary Plan*

The heritage values of the wall are assessed below against the categories of historic heritage value for the place identified in the Auckland Unitary Plan. These are then summarised in a statement of significance. Criteria that contribute to the heritage values of the place are assessed as being:

Historical:

The place reflects important or representative aspects of national, regional or local history, or is associated with an important event, person, group of people or idea or early period of settlement within the nation, region or locality.

The brick wall on the perimeter of the Oakley Hospital Main Building / former Carrington Hospital was constructed in 1887 and formed part of an early extension to the original asylum building that was constructed in 1867 and partially rebuilt following a fire in 1877. The perimeter wall enclosed an area of outdoor containment known as an 'airing court' where female patients had access to fresh air and sunlight. The wall formed part of the main hospital building and is illustrative of early methods of care and treatment of psychiatric patients in New Zealand.

Mana Whenua:

The place has a strong or special association with or is held in high esteem by Mana Whenua for its symbolic, spiritual, commemorative, traditional or other cultural value.

The wall has not been assessed under this criterion. However, the wider hospital site is known to have significance to Māori as a pre-European landscape.

Technology:

The place demonstrates technical accomplishment, innovation or achievement in its structure, construction, components or use of materials.

The wall demonstrates the use of locally made bricks and may have used cement from the Wilsons Cement Works at Warkworth.

Physical Attributes:

The place is a notable or representative example of a type, design or style, method of construction, craftsmanship or use of materials or the work of a notable architect, designer, engineer or builder.

The wall was designed by the Government Architect, Charles Beatson, as part of additions to the main asylum building, which were designed in the Italianate style and carefully detailed to match the original building. The wall is arranged into rectangular sections with pilasters and displays a similar consideration for aesthetics as the main building.

Aesthetic:

The place is notable or distinctive for its aesthetic, visual or landmark qualities.

Although now separated from the Oakley Hospital Main Building, the wall when built was originally connected to its exterior walls and was a distinctive feature of 1887 additions made to the building's east wing. The wall enclosed an 'airing court', which securely contained female patients while they accessed fresh air and sunlight.

Context:

The place contributes or is associated with a wider historical or cultural context, streetscape, townscape, landscape or setting.

The remaining wall is associated with the wider history of the Oakley Hospital Main Building / former Carrington Hospital including its growth over time to address ongoing issues of overcrowding during the late 19th century. The wall provides an insight into the way in which people with mental illness were accommodated in the late 19th century.

SUMMARY

The brick wall that is the subject of this report formed part of an 'airing court' built to contain patients at Auckland's purpose-built asylum. The wall was designed and constructed during additions to the asylum's eastern female wing and contained patients while allowing them to have access to fresh air and sunlight.

The perimeter wall has remained largely intact since this time and provides a memory of the way in which New Zealanders suffering mental illness were accommodated in the 19th century. The wall was original connected to the eastern wing of the main building but has subsequently been modified so that it now reads as an independent structure. It nevertheless can be interpreted as a part of the wider hospital site.

PART THREE: IMPACT ON HERITAGE VALUES

8 ASSESSMENT OF IMPACT ON HERITAGE VALUES

8.1 IMPACT ON HERITAGE VALUES

In the following table, the impact on heritage fabric and consequential impact on the heritage values of the Carrington site is assessed.

Description of Work	Impact on Heritage Fabric	Impact on Heritage Values of Oakley Main Hospital Building extent of place
Demolition of existing brick wall adjacent to western footpath of Carrington Road within Oakley Main Hospital Building extent of place.	A section of this wall was built in 1887. The removal of the wall will result in the loss of heritage fabric.	<p>The 1887 Airing Court brick wall was built during additions to the east wing of the main hospital building in 1887. However, it has become disassociated from the main hospital building as a result of further modifications, including the apparent removal of sections of the wall that returned at right angles to connect with the eastern wing.</p> <p>The wall is also currently mostly obscured by dense vegetation, however, it nevertheless forms part of the story of the ongoing development and expansion of the hospital building during the late 19th century.</p> <p>The wall's removal will provide opportunities for additional green space to be developed on the western side of Carrington Road and will create opportunities for the heritage values of the main hospital building to be more readily appreciated by being more visible from the road.</p> <p>With the brick wall comprising only a small part of the former hospital it is considered that any negative impact on the heritage values of the place as a result of its removal will be minor.</p>
Removal of other buildings including the Occupational Therapy Training School and the staff laundry. Removal of a later boundary fence.	Neither building nor the boundary fence are considered to have heritage value.	The removal of the buildings in question and the boundary fence will not impact on the heritage values of the place. Note that the removal of the Occupational Therapy Training School and staff laundry will be demolished prior to the construction of the Project as part of the Carrington Residential Development works (i.e. only the removal of the boundary fence will be undertaken as a part of the Project).
Provision of new infrastructure including bus stop, paving, new grass berms, streetlights, trees, services, watermain & below ground valve chamber.	Provision of new infrastructure	<p>The provision of new infrastructure within the Extent of Place will also impact on the heritage values of the place. Much of the infrastructure including grass berms and paving is mostly at ground level or beneath the ground and its impact on the heritage values of the place will be less than minor.</p> <p>The bus stop, by its nature, will be more visible and therefore have greater impact, depending on its final design. It is noted that there is a bus stop with a basic shelter in a location close to where the new bus stop will be. If the new bus stop is similar to the existing bus stop, it will have a minor impact on the heritage values of the former Carrington Hospital.</p>

8.2 POSSIBLE MITIGATION

It is considered that the removal of the Airing Court brick wall will have a minor impact on the heritage values of the former Carrington Hospital. Other proposed changes including the provision of the new infrastructure are also likely to have a minor or less than minor impact on heritage values.

It is noted that the documentation for the work is currently only at the preliminary design stage. As such, the impact of the changes cannot be fully assessed until the design is finalised. This applies particularly to the bus stop and any shelter that might be incorporated, which arguably will be the most visible element of the proposed works.

Accordingly, the proposed conditions of consent require further design and mitigation detail to be provided for works within the extent of place prior to construction – in particular through a Urban Design and Landscape Plan (ULDP), and a Heritage Construction Management Plan (HCMP).

There are numerous actions that could be considered to mitigate the impact of the changes on the heritage values of the place and some of these are outlined in the following section.

Salvage of Bricks

As noted, the Airing Court wall was constructed in 1887. The bricks will obviously be at least that old and probably made locally as there were literally dozens of brickworks dotted around Auckland from the mid nineteenth century through to the early years of the twentieth century. Many of these were located along the banks of the Whau River, a short distance from the hospital site.

Every effort should be made to salvage a quantity of bricks from the Airing Court wall for possible reuse. Some bricks have previously been salvaged from later parts of the main building when these were recently demolished. Mortar used for brickwork around the 1880s was typically manufactured by burning seashells and was generally soft and of poor quality. The mortar used in the main building, however, was surprisingly hard, suggesting that cement was used in its manufacture. Consequently, the mortar proved somewhat difficult to remove. There may be similar issues with the bricks from the Airing Court wall if cement was also used in its construction.

Cement was being produced by J Wilson and Company in Warkworth from around 1885, so if local cement was being used, as opposed to imported cement, it would be one of the earliest uses of the products in New Zealand.

Uses for which bricks salvaged from the Airing Court wall could include the following:

- **Brick Paving**

Part of the proposed cycle way where it passes the end of the Carrington building appears to follow the line of the existing wall. Some of the bricks could be retained and cleaned and relaid along this line.

- **Replacement Walls/Fences**

Early photographs of the asylum such as those reproduced on pages 7 and 17 show that the site originally had very simple timber post and rail boundary fences and a suggestion has been made that this style of fencing could be repeated along the new boundary line between Carrington and the adjacent widened road.

Alternatively, a section of the existing brick wall could be reconstructed along the new boundary line, reusing some of the existing bricks.

- **Interpretive Panel/s**

Interpretive panel/s should be provided detailing the history of the site with plans and photographs highlighting the demolished wall. A supply of bricks from the wall could be retained and used to construct a plinth on which the interpretive panel/s could be mounted.

- **Bus Stop**

As noted, there is a rudimentary bus shelter on the roadside, a short distance away from where a new shelter is proposed. Although it is not proposed that a “Carrington Heritage” bus shelter be constructed, there may be an opportunity to reuse some of the salvaged bricks in its construction.

It is noted that the proposed bus shelter is shown as being within the Carrington Hospital Extent of Place. A resource consent will therefore be required, being a new structure within an Extent of Place as specified in **D17.4.1 Activity Tables (A10)** of the Auckland Unitary Plan.

9 PROVISIONS OF THE AUCKLAND UNITARY PLAN

9.1 ACTIVITIES WITHIN THE EXTENT OF PLACE

The Oakley Hospital Main Building at 1 Carrington Road, Mount Albert has been identified in the Auckland Unitary Plan Schedule 14.1 Schedule of Historic Heritage as a Category A historic heritage place (listing number 01618). This means that it has outstanding significance well beyond its immediate environs. The listing excludes all buildings constructed after 1905, whether attached to the Oakley Hospital Main Building or freestanding, all vegetation within the extent of place and all post 1905 modifications to the interior of the building. The area of the building that is identified as the primary feature is shown on page 26.

The historic airing court brick wall along Carrington Road is not specifically included as part of the scheduled item but is within the associated extent of place. The proposed work will require removal of the airing court wall in its entirety. Also required to be removed is a low level brick wall which extends northwards from the airing court wall as far as SH16. This wall appears to be of later construction. The airing court wall and the later wall can both be described as features within an extent of place.

New infrastructure associated with the road widening including new paving will also be built within the Extent of Place.

9.2 AUCKLAND UNITARY PLAN POLICIES

Section D17.3 Policies of the Auckland Unitary Plan includes the following policies related to the *Demolition or destruction* of historic heritage places:

(12) <i>Avoid the total demolition or destruction of the primary features of Category A scheduled historic heritage places.</i>
Not applicable. The wall is not a primary feature of a Category A scheduled historic heritage place.
(13) <i>Avoid the total or substantial demolition or destruction of features (including buildings, structures or archaeological sites) within scheduled historic heritage places where it will result in adverse effects (including cumulative adverse effects) on the overall significance of the scheduled historic heritage place to the extent that the place would no longer meet the significance thresholds for the category it has been scheduled.</i>
The proposed demolition of the wall will not result in the Oakley Main Hospital Building no longer meeting the significance thresholds for a Category A historic heritage place. The wall is an independent structure and no longer connected to the main building.
(14) <i>Avoid the total or substantial demolition or destruction of:</i> <i>(a) the primary features of Category A* and Category B scheduled historic heritage places;</i> <i>(b) the non-primary features of Category A and A* scheduled historic heritage places; and contributing features within Historic Heritage Areas; unless:</i> <i>(i) the demolition or destruction is required to allow for significant public benefit that could not otherwise be achieved; and</i>
The demolition of the wall is required to enable road widening to accommodate a new raised cycle lane that will enhance the safety of cyclists in a heavily used section of roadway close to the northwestern cycle path. A bus lane will also be accommodated. There will be an improvement in public safety and a significant public benefit as a result of upgrading the cycle lane and creating a bus lane.
<i>(ii) the significant public benefit outweighs the retention of the feature, or parts of the feature, or the place; or</i>

<p>The existing brick wall is a modified version of the original 1887 wall and is currently largely obscured by vegetation. The wall is no longer attached to the Oakley Main Hospital Building and reads as a separate element. The significant benefit to the public of a safer cycle lane outweighs the retention of the heritage wall.</p>
<p><i>(iii) the demolition or destruction is necessary to remove a significant amount of damaged heritage fabric to ensure the conservation of the scheduled historic heritage place.</i></p>
<p>Not applicable.</p>
<p><i>(15) Enable the total or substantial demolition or destruction of features (including buildings, structures or archaeological sites) where:</i></p> <ul style="list-style-type: none"> <i>(a) it is established that the feature detracts from the heritage values of a scheduled historic heritage place;</i> <i>(b) the feature is identified as a non-contributing feature within a scheduled Historic Heritage Area; or</i> <i>(c) the feature is identified as an exclusion in Schedule 14.1 Schedule of Historic Heritage.</i>
<p>Not applicable.</p>
<p><i>(25) Enable the establishment of network utilities and small-scale electricity generation facilities within scheduled historic heritage places where all of the following apply:</i></p> <ul style="list-style-type: none"> <i>(a) there is a functional need or operational constraint that necessitates their location within a scheduled historic heritage place;</i>
<p>The need to locate the proposed works within the extent of place is required by a functional need and operational constraints. Further narrowing of the proposed corridor would not achieve the transport outcomes sought and negate the significant public benefit of the Project; and widening on the opposite (eastern) side of the road would require the designation of third party land and removal of several established Pohutukawa trees.</p>
<ul style="list-style-type: none"> <i>(b) significant adverse effects on the heritage values of the place are avoided where practicable; and</i> <i>(c) Other adverse effects are avoided, remedied, or mitigated</i>
<p>The assessment has concluded that the effects on the heritage values of the place are minor and can be further mitigated through the recommended measures. Accordingly, significant adverse effects on heritage values will be avoided; and other adverse effects will be managed as appropriate.</p>
<p><i>(26) Avoid the relocation and or substantial demolition or destruction of features within a scheduled historic heritage place to provide for network utilities and electricity generation facilities unless all of the following apply:</i></p> <ul style="list-style-type: none"> <i>(a) a functional need or operational constraint limits available alternatives;</i> <i>(b) there is no reasonable practicable alternative</i>
<p>The need to locate the proposed works within the extent of place is required by a functional need and operational constraints which in effect mean there is no reasonable practicable alternative. Further narrowing of the proposed corridor would not achieve the transport outcomes sought and negate the significant public benefit of the Project, and was ruled out as an alternative. Widening on the opposite (eastern) side of the road would require the designation of third party land and removal of several established Pohutukawa trees, and was also ruled out as an alternative.</p>
<ul style="list-style-type: none"> <i>(c) the infrastructure will provide a significant public benefit that could not otherwise be achieved</i>
<p>The demolition of the wall is required to enable road widening to accommodate a new raised cycle lane that will enhance the safety of cyclists in a heavily used section of roadway close to the northwestern cycle path. A bus lane will also be accommodated.</p>

There will be an improvement in public safety and a significant public benefit as a result of upgrading the cycle lane and creating a bus lane.
<i>(d) the adverse effects on the heritage values of a place are minimised to the extent practicable.</i>
The assessment has concluded that the effects on the heritage values of the place are minor and can be further mitigated through the recommended measures. Localised narrowing has ensured the primary feature within the extent of place is avoided. Accordingly, adverse effects on heritage values of the place will be minimised to the extent practicable.

9.3 D17.4 ACTIVITY TABLES

Section D17.4.1 of the Auckland Unitary Plan comprises an The Activity Table which specifies the activity status of land-use activities within the historic heritage overlay requiring resource consent.

(A1) Demolition or destruction of 70% or more by volume or footprint (whichever is the greater) of any feature

The demolition or destruction of 70% or more of features within the scheduled extent of place of Category A and A* places are considered to be a Non-Complying (NC) Activity. The removal of the brick walls falls into this category and this work is therefore considered to be a non-complying activity.

(A9) Modifications to, or restoration of, buildings, structures, fabric or features of a scheduled historic heritage place, except where provided for as a permitted, controlled or restricted discretionary activity in another rule in this overlay

Modifications to fabric within the scheduled extent of place of Category A and A* places are considered to be a Restricted Discretionary Activity. This includes the provision of new infrastructure within the Extent of Place.

(A10) New buildings or structures. The proposed bus stop falls within the definition of a new building as it is outside of the current road reserve, and is considered to be a Discretionary Activity within a Category A extent of place.

Additionally, resource consent is required under the infrastructure chapter provisions (activity tables **E26.6.3.1** and **E26.8.3.1**) for the following activities:

(A115) Earthworks for network utilities and electricity generation facilities within a Historic Heritage and Special Character area that do not comply with the permitted activity standards in E26.6.5.2 are considered to be a Restricted Discretionary Activity. The standards are infringed as earthworks are proposed to take place within 20m of the main hospital building.

(A131) Network utilities and electricity generation facilities not otherwise provided for are considered to be a Restricted Discretionary Activity for which a resource consent will be required. While some of the proposed works are permitted activities under Table E26.8.3.1 (for example activities A126-128 identify certain road network activities within the historic heritage overlay as being permitted), some of the proposed works are not provided for under Table E26.8.3.1.

10 HERITAGE NEW ZEALAND POUHERE TAONGA

10.1 LISTING OF FORMER CARRINGTON HOSPITAL

The former Carrington Hospital is listed by Heritage New Zealand Pouhere Taonga as a Category 1 Historic Place. The extent of the listing is not defined on the HNZPT website, although the site has a legal description of Lot 5 DP 314949 (RT 58983), North Auckland Land District.

The site is also understood to be an archaeological site which is defined in the *Heritage New Zealand Pouhere Taonga Act 2014* as a place associated with pre-1900 human activity, where there is evidence relating to the history of Aotearoa New Zealand.

Under the Act, an Archaeological Authority is required where a pre-1900 building is being demolished or modified. It is noted, however, that recording is not required for a pre-1900 building unless it is to be demolished in whole.

Heritage New Zealand Pouhere Taonga has been consulted regarding the wider project and, in particular, the need to demolish the Airing Court wall.

10.2 ARCHIVAL RECORDING

Theoretically, the wall is not required to be recorded under the HNZPT Act as the listed item is not being demolished in whole. Nevertheless, Auckland Council has requested that the Airing Court wall be subject to archival recording prior to its removal.

Heritage New Zealand Pouhere Taonga has produced a series of archaeological guidelines including one entitled *Investigation and recording of buildings and standing structures*. This publication outlines the requirements for recording a building or structure. Three levels of recording are outlined that correspond to the archaeological value of a building or structure as follows:

- Level 1 - This level is applicable as a minimum for recording for a building or structure of high archaeological value. its removal.
- Level 2 - This level is applicable as a minimum for recording of a building or structure of medium archaeological/ heritage value.
- Level 3 - This level is applicable as a minimum for recording of a building or structure of low archaeological/ heritage value.

Each level of recording includes photography, drawings or sketches and a written description of the building or structure. The higher levels of recording require more extensive photography, drawings and written descriptions.

In this case, Heritage New Zealand Pouhere Taonga should be consulted to determine the level of recording that would be appropriate in this situation.

11 SUMMARY AND CONCLUSION

The demolition of the 1887 Airing Court brick wall and the later low-level brick wall within the Oakley Main Hospital Building extent of place is a non-complying activity as assessed under Section D17.4.1 of the Auckland Unitary Plan. However, the removal of the wall will result in significant public benefits resulting from the creation of a new and safer cycle lane and a bus lane on the western side of Carrington Road. Similar improvements will also be made to the eastern side of Carrington Road.

Policies in Section D17.3 of the Auckland Unitary Plan allow for the demolition of features within the scheduled extent of place of a Category A historic heritage place if it will result in significant public benefit that could not otherwise be achieved.

Carrington Road is an arterial route with increasing traffic demands and the provision of safe cycle lanes and bus lanes will provide significant public benefits. Alternative proposals to enable the road to be widened while still retaining the airing court brick wall were explored. These included widening the road on the eastern side, without which there would be insufficient capacity to accommodate the proposed improvements. Widening the road on the eastern side would require additional land to be designated, which is likely to be problematic, considering land on the western side of Carrington Road has already been designated for the purpose.

Widening the road on the eastern side of Carrington Road would likely require the removal of some or all of a group of mature Pohutukawa trees that are well established. This activity would affect the character of the surrounding area. Any designation requirements on the eastern side of the road would also affect a number of private properties which could attract opposition.

The proposed demolition of the Airing Court wall is therefore required and this could result in a minor impact on the overall heritage values of the Carrington site.

The impact on the heritage values of the former hospital and its Extent of Place as a consequence of the demolition of the wall could be partly mitigated if certain measures were implemented. Specifically, these might include the provision of an interpretation panel or panels outlining the history of the place and the Airing Court wall in particular. The interpretive panels could be mounted on a brick plinth and to this end, a quantity of bricks from the wall should be salvaged.

The bricks could also be used to provide a section of paving or possibly to recreate a section of the Airing Court wall.

Such work as the construction of the interpretive panels or recreating a section of the wall is likely to be counted as new buildings or structures within an extent of place and the impact of such work on the heritage values will need to be assessed. It is noted that interpretive panels, fences, paving and the items that are being suggested as a way of mitigating the loss of the Airing Court wall have yet to be designed and their impact is unable to be assessed until this has occurred.

Provision of new infrastructure including the bus stop, grass berms, streetlights, trees, services and new watermain with its below ground valve chamber will also require a resource consent where it is within the extent of place. The bus shelter has yet to be designed and its impact will also need to be assessed. The shelter should, however, be devoid of any advertising to minimise any negative impact.

Prior to the removal of the brick walls and the building of new infrastructure within the Extent of Place, a Heritage Construction Management Plan should be put in place to minimise the possibility of any damage occurring to the former Carrington Hospital main building.

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WEBSITES

Auckland Council

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13 ADDENDUM

In Section 8.2 of the original Heritage Impact Assessment, it was suggested that certain measures could be put in place as a way of mitigating in part the loss of the airing court wall. Some of these measures have now been included in the documentation for the widening of Carrington Road, which is attached as 13.4. These measures include the following.

13.1 PROPOSED MITIGATION

Reuse of Bricks

The bricks of which the airing court wall is comprised date from 1877 or earlier, being the time the eastern wing of the former hospital was constructed. Although it appears that cement mortar was used in the construction of the wall, elsewhere on the site where brickwork has been dismantled, it appears that bricks can be salvaged for reuse.

Accordingly, it is now proposed to reuse bricks from the wall and to incorporate them into various features in the landscape. Specifically, it is proposed to create a garden area at the end of the former hospital wing that extends out into the area set aside for new infrastructure. Around this garden, salvaged bricks will be used to create a border.

Interpretive Panel

Within the same area, a plinth will be constructed from salvaged bricks, onto which will be mounted an interpretive panel. The final design of the panel has yet to be established but it is likely that it will include a brief history of the hospital and information regarding the demolished airing court wall. Salvaged bricks will also be used to create a small, paved area in front of the bricks where people can pause to read the plaque.

Bus Shelter

The proposed bus shelter will have a series of glazed panels around its perimeter. On these glazed panels, it is proposed to provide images that may include historical photographs of the former Carrington Hospital.

13.2 UNITARY PLAN PROVISIONS

New buildings or structures. The proposed plinth is likely to be deemed to be a new structure as it will be located within the Extent of Place. It will therefore be considered to be a Discretionary Activity and a resource consent will need to include this item.

13.3 IMPACT ON HERITAGE VALUES

It is considered that these measures, being the reuse of the bricks from the wall, the provision of the interpretive panel on a brick plinth and the provision of heritage images on the glazed panels in the bus shelter, will have a positive impact on the heritage values of the place. They will also go some way to mitigating the loss of the airing court wall through the reuse of bricks from the wall and the provision of information regarding the earlier form of the place.

13.4 DRAWINGS OF PROPOSED WORKS

Refer to attached drawings by Beca and Boffa Miskell showing proposed mitigation opportunities



CARRINGTON ROAD IMPROVEMENTS PROJECT




HERITAGE INTERPRETATION AND MITIGATION

RESOURCE CONSENT INFORMATION PACK
APRIL 2025

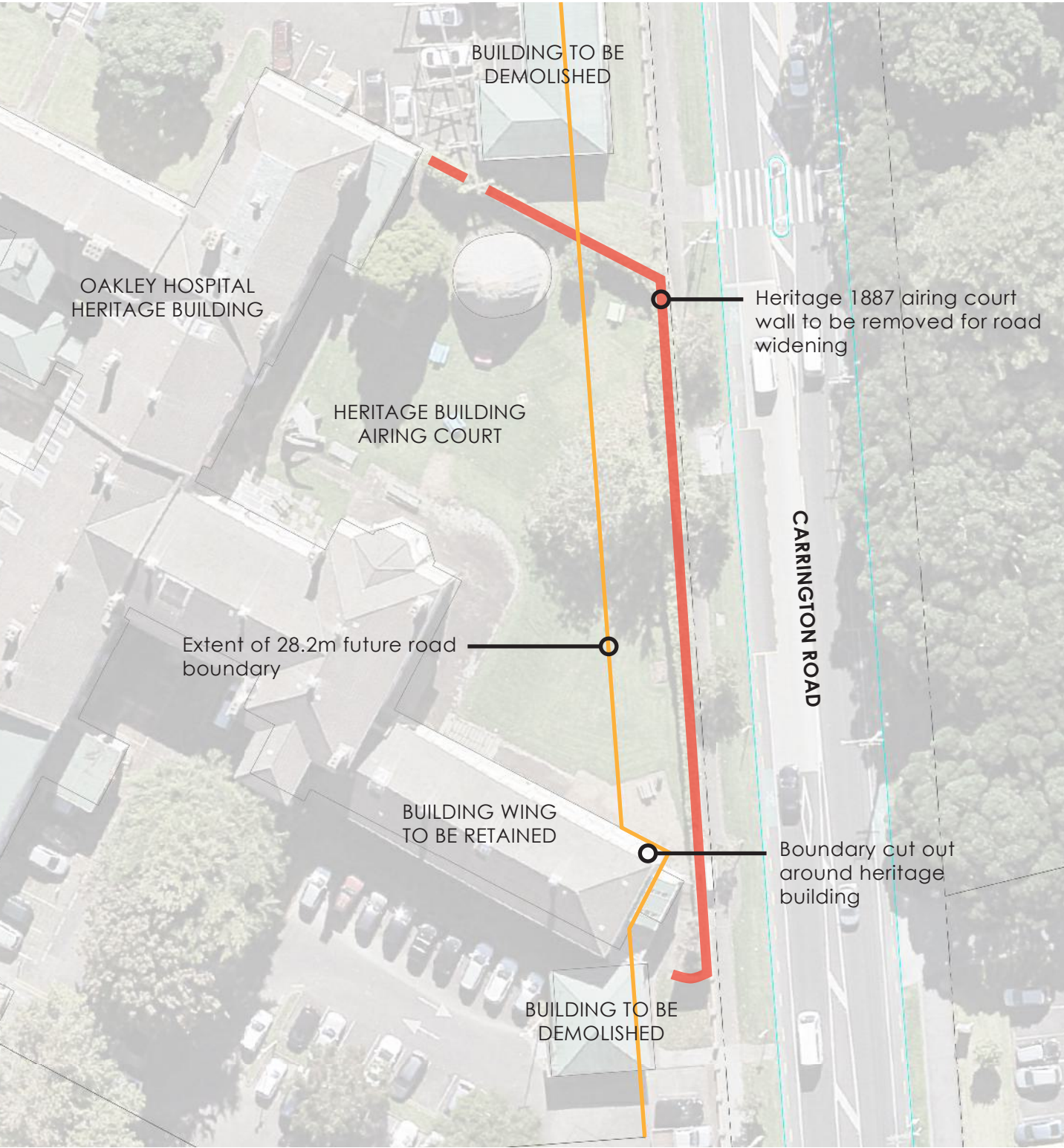
HERITAGE CONTEXT

PROJECT CONTEXT

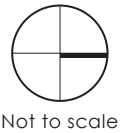
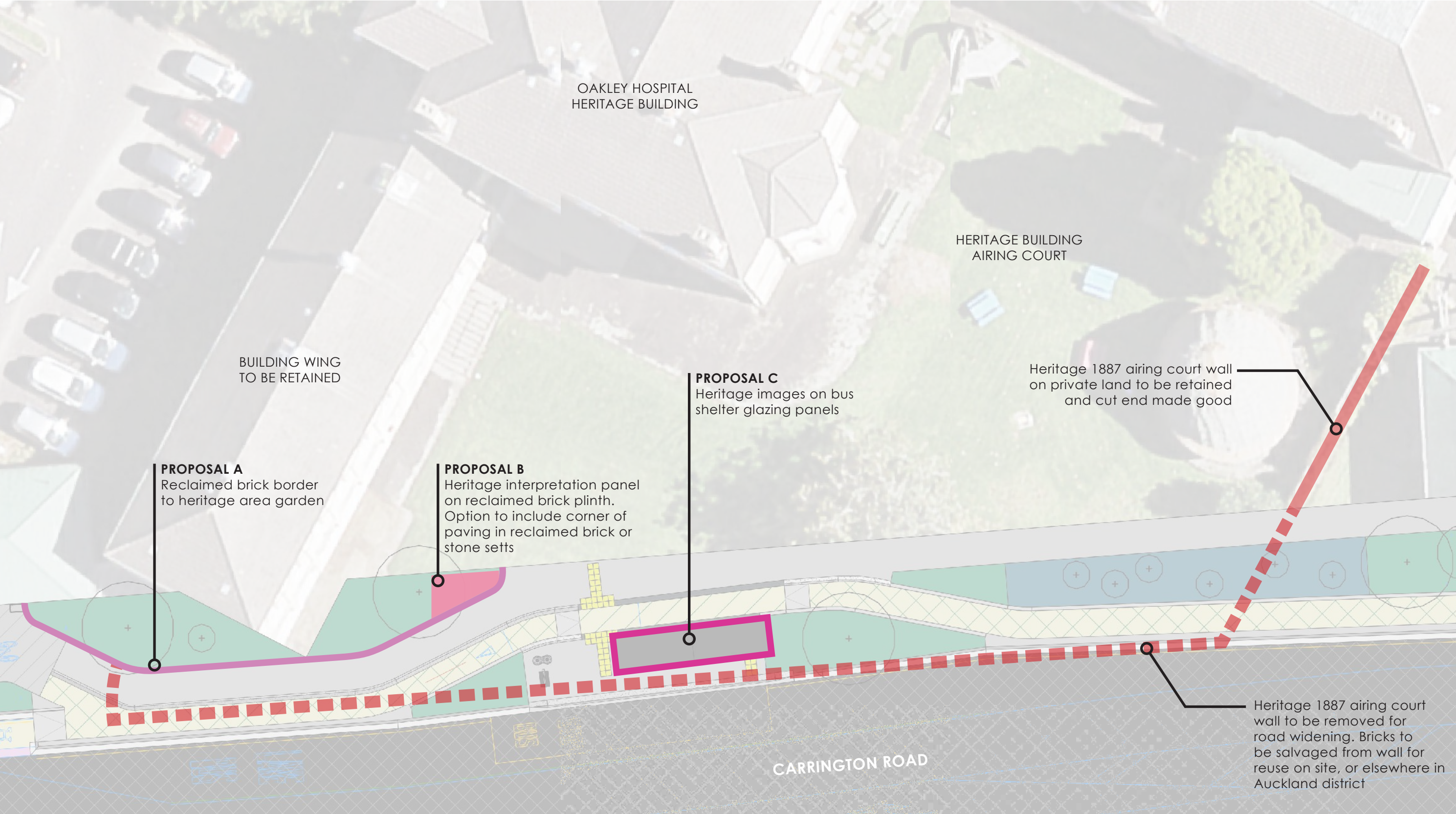


-  HOSPITAL BUILDING
-  CARRINGTON RD
-  FOCUS AREA

SCOPE OF ALTERATIONS

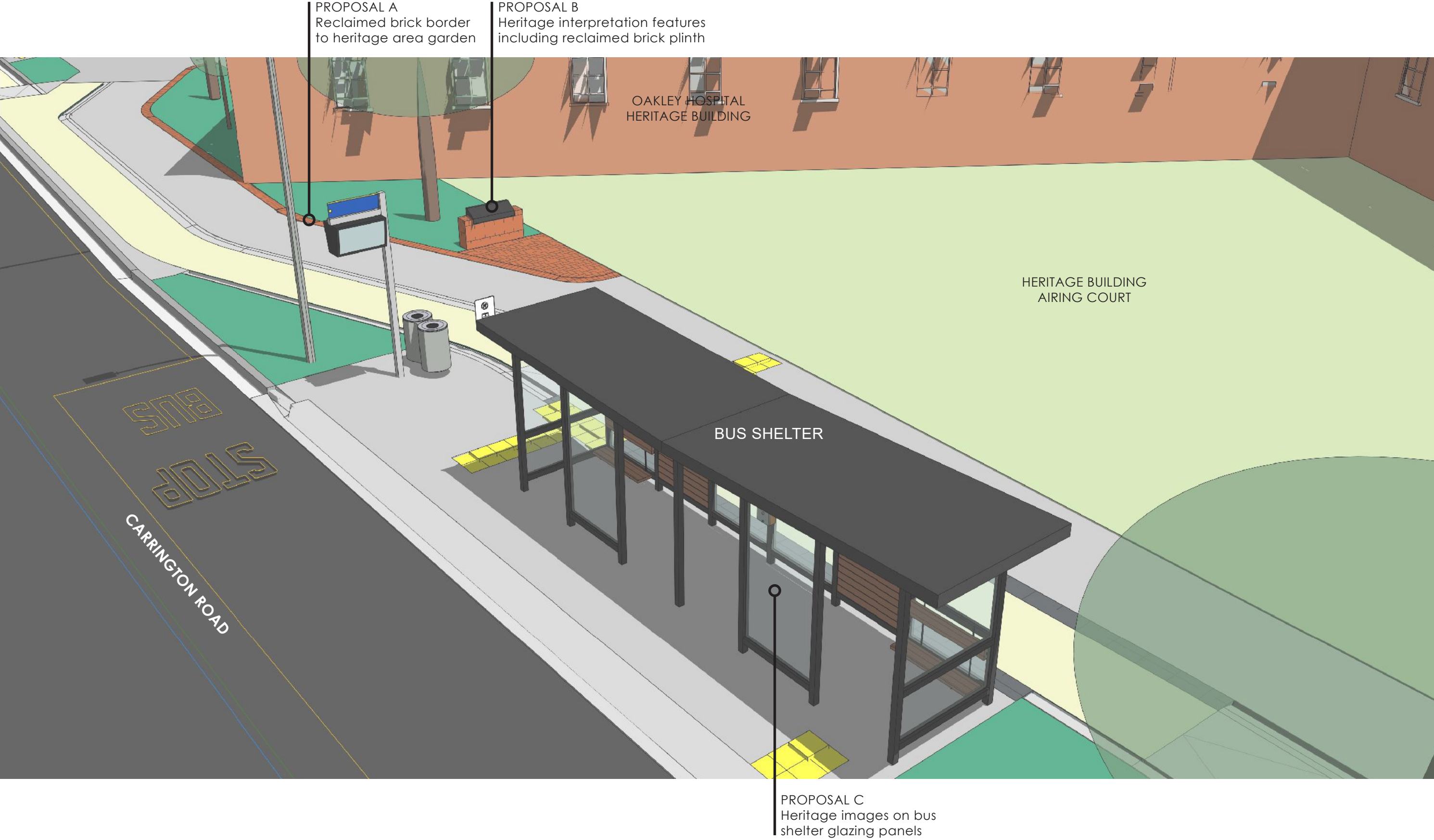


PROPOSED STREET DESIGN & MITIGATION OPPORTUNITIES



Not to scale

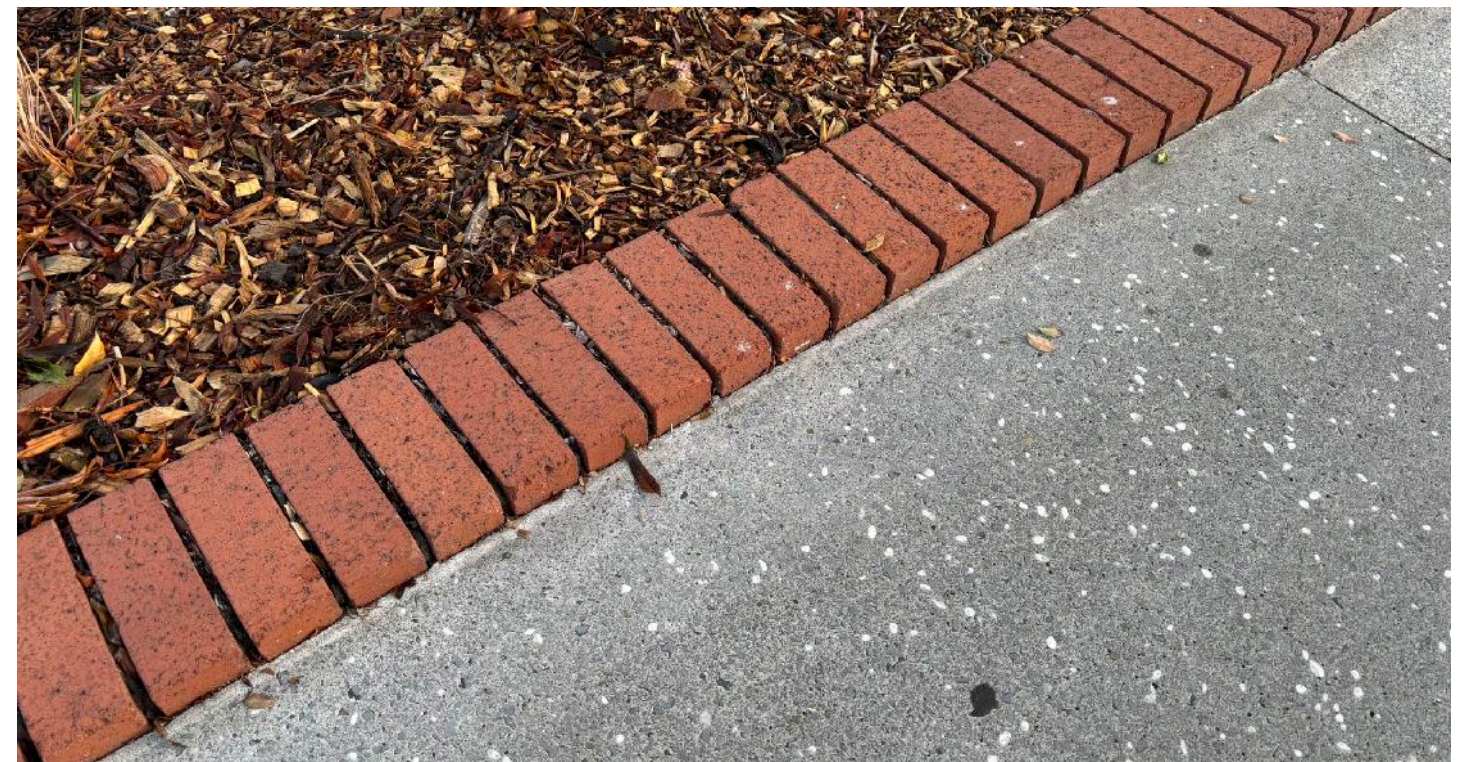
PROPOSED STREET DESIGN & MITIGATION OPPORTUNITIES



PROPOSAL A - RECLAIMED BRICK EDGE TO GARDEN



- Existing heritage Airing Court brick wall (above) to be removed to enable road construction
- Bricks likely sourced from Avondale (Rosebank) or New Lynn Brickworks
- Brick condition observed to be varied but suitable for reuse if careful demolition methodology is utilised
- Brick surfaces are rough and slippery when wet making them unsuitable for use in footpath
- Brick dimensions vary but average 210-220mm L x 105mm W x 75mm H



Bricks may be reused within the design in the following ways:

- Brick edging to garden beds to retain mulch / soil
- Low brick walls to manage level changes between heritage building and footpath
- Paving in heritage interpretation areas outside of the main footpath

These are illustrated via the examples above in New Lynn Town Centre

PROPOSAL B - HERITAGE INTERPRETATION PLINTH



- Reclaimed brick plinth with interpretation panel, located to oneside of the footpath clear of main path of travel
- Corner area could be paved in reused brick or stone setts



- Reclaimed brick may be used to create a low wall / plinth to house a heritage panel and to represent the former brick wall
- Indicative wall dimensions:
- A heritage panel may be designed in collaboration with Auckland Council to tell the story of the Oakley Hospital Building, and its subsequent life as a Unitec campus. This could be a small plaque or a larger interpretation sign

PROPOSAL C - BUS SHELTER GLAZING PANELS

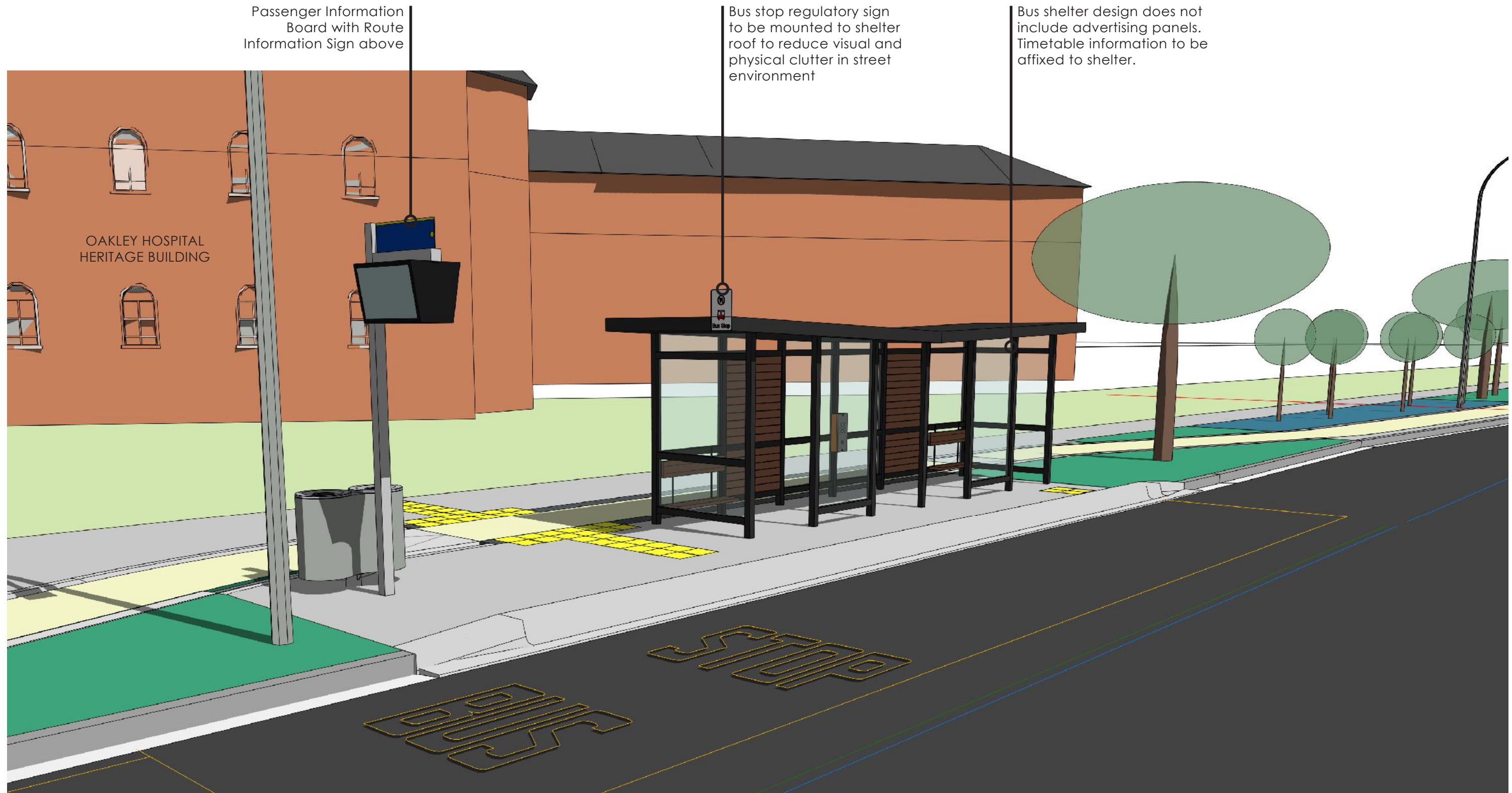


- A large canvas exists on the glazing panels of the bus shelter adjacent to the Oakley Hospital heritage building
- Artworks or heritage images must accommodate CPTED & sightline requirements



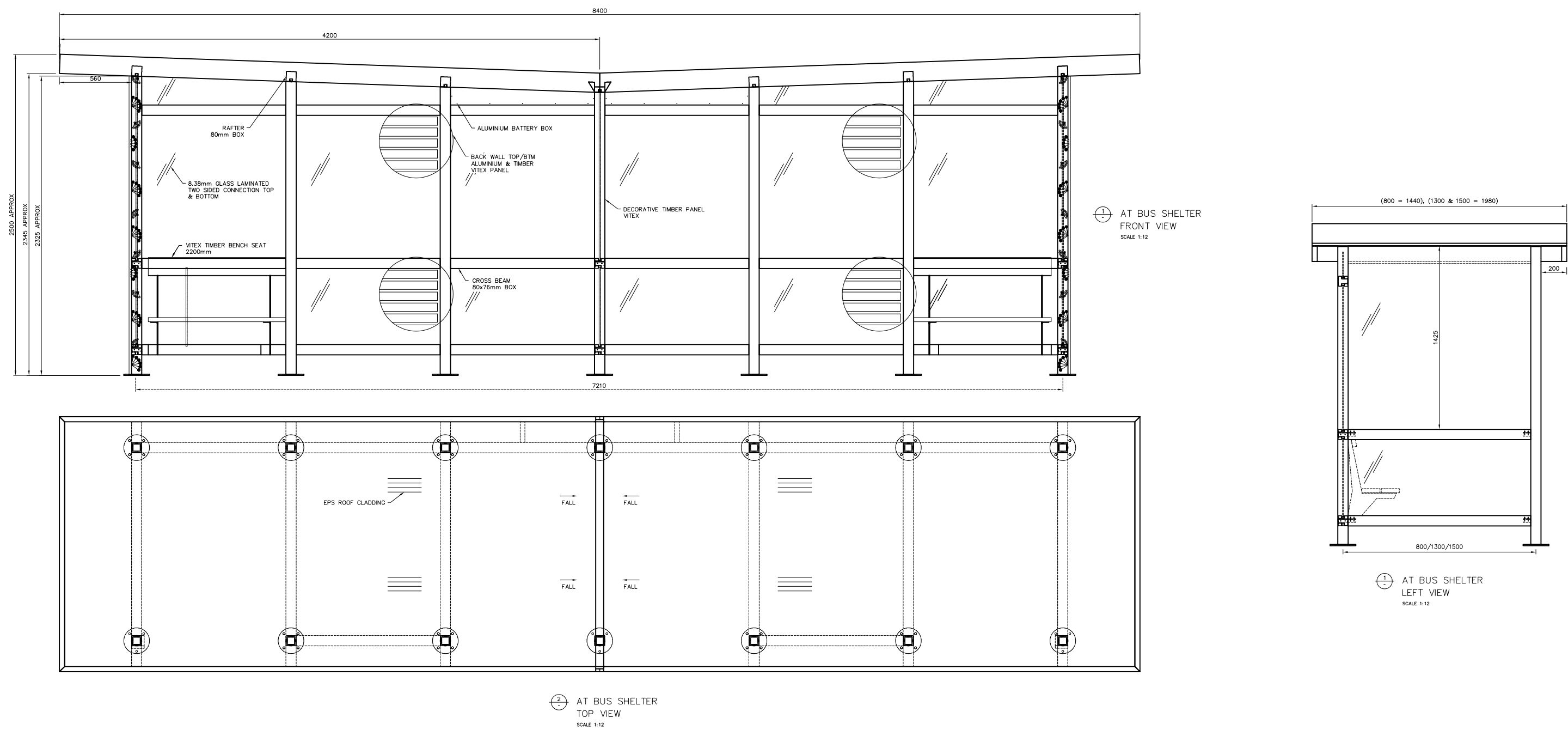
- Heritage images or an interpretive artwork could be inexpensively applied to these glazing panels or other surfaces as has been done elsewhere such as at Hobsonville Point to connect to the history of the place

PROPOSAL C - BUS STOP SIGNAGE



- Bus stop signage will be limited to bus stop information signage. No advertising signage is proposed to be included in the bus stop (which is why consent is not sought under E23.4.2(A50)).

PROPOSAL C - BUS STOP SHELTER DESIGN



AUCKLAND TRANSPORT INTERMEDIATE SHELTER DRAWINGS

Source: ARA Shelters Shop Drawing